

April 27, 2022

Adam Sheffer Partner **Originate Build** E-Mail: adam@originate.build

RE: 152-164 Bathurst Street and 621-627 Richmond St West Updated Transportation Impact Study / Response to City of Toronto Comments

Dear Adam:

The following transportation content is an update as part of the Site Plan Approval (SPA) application #1 and Zoning By-law Amendment (ZBA) application resubmission (Application No. 21 181257 STE 10 OZ) being made to the City of Toronto to address Engineering Construction Services department comments.

This letter addresses all comments received, outlines the transportation demand management plan for 152-164 Bathurst Street and 621-627 Richmond Street West (herein referred to as the "site" or "Bathurst / Richmond"), and updates the parking strategy for the current SPA #1 submission.

1.0 INTRODUCTION

BA Group is retained by Originate Developments Inc. to review the transportation aspects of a Site Plan Application (SPA) being made to the City of Toronto in relation to proposed development of 152-164 Bathurst Street and 621-627 Richmond Street West located at the corner of the intersection of Richmond Street West and Bathurst Street in the City Toronto.

A Site Plan Application has been made to the City of Toronto to permit the development of 152-164 Bathurst Street and 621-627 Richmond Street West. The proposed development comprises of a 17-storey with rooftop, mixed-used development including 216 residential units and 528 m² of at-grade retail uses.

A Zoning By-law Amendment Application was made to the City of Toronto staff in June 2021. The latest transportation-related comments provided by City staff on the application are from Engineering and Construction Services, dated November 9, 2021.

The current SPA #1 submission is similar to the previous June 2021 ZBA submission with minor changes including the reduction of 1 unit, a 6 m² retail GFA increase, 1 parking space increase, and a 21 bicycle parking space (19 long-term and 2 short-term) decrease as summarized in Table 1.

As a result, the trip generation and assignment based on the current development programme is generally consistent with the June 2021 ZBA submission, which Transportation Services accepted the methodology and conclusions of the submitted ZBA report as it relates to vehicular traffic. Therefore, the same methodology and conclusions are applicable to the current SPA #1 submission and are subsequently, reasonable and approvable by the City.

1.1 DEVELOPMENT STATISTICS

A comparison between the original and current site statistics is summarized in **Table 1**. Reduced scale architectural plans and landscape architectural plans are attached in **Appendix A** and **Appendix B**, respectively.

Land Use	June 2021 ZBA #1 Submission	April 2022 SPA #1 Submission	Net Change ZBA #1 to SPA #1
Residential	217 units	216 units	-1 unit
Retail	522 m² GFA	528 m² GFA	+6 m ²
Parking Supply	69 spaces (51 resident) 0.23 spaces/unit (18 commercial) 0.08 spaces/unit	70 spaces (52 resident) 0.24 spaces/unit (18 commercial) 0.08 spaces/unit	+1 space
Loading	1 Type 'G'	1 Type 'G'	No change
Bicycle Parking	244 spaces (217 long-term) (27 short-term)	223 spaces (198 long-term) (25 short-term)	-21 spaces (-19 long-term) (-2 short-term)



2.0 RESPONSE TO COMMENTS

Transportation related comments from City of Toronto Engineering and Construction Services department are outlined in *italics* below with responses from BA Group.

2.1 RESPONSE TO ENGINEERING & CONSTRUCTION SERVICES (DATED NOVEMBER 9, 2021)

A. REVISIONS AND ADDITIONAL INFORMATION REQUIRED FOR PLANS STUDIES AND DRAWINGS

Comment to Part A.1.1

In consultation with Cycling and Pedestrian Projects, design and construct a raised bicycle lane along the south side of Richmond Street West, extending from the site driveway to Bathurst Street, at a minimum width of 1.8 metres. This lane is to be constructed in accordance with the City's T-990.100 design specification. Please contact Becky Katz at becky.katz@toronto.ca for more information.

Response to Part A.1.1

Noted. A raised bicycle lane along the south side of Richmond Street West, extending from the site driveway to Bathurst Street, at a minimum width of 1.8 metres has been proposed in accordance with the City's T-990.100 design specification. Please refer to the updated ground floor plan provided in **Appendix A**.

Comment to Part A.1.3

Provide further documentation and review the functionality (including acceptable vehicle manoeuvring diagrams) of the proposed substandard or "small-car" parking spaces.

Response to Part A.1.3

Noted. Please refer to BA Group's SPA #1 transportation report and the small car review provided in **Appendix B**, which illustrates the functionality of the proposed substandard parking spaces.

D. BACKGROUND

Comment to Parking

The provided report notes that up to 10 percent of the provided parking spaces are expected to be substandard, with minimum dimensions of 2.4 metres wide, 5.1 metres long and 1.7 metres high. This is acceptable in principle, but prior to Site Plan Control approval, the applicant will need to identify these spaces and review the functionality of their design and use. This will require further documentation and the submission of acceptable vehicle manoeuvring diagrams (VMDs). In addition, the applicant is advised that prior to the release of the Draft Plan of Condominium for registration, acceptable documentation must be included advising future owners of the proposed substandard or small-car parking space dimensions and that they are not meant for all vehicle types and would be best served as small-car spaces only.

Response to Parking

Noted. Please refer to BA Group's SPA #1 transportation report and the small car review provided in **Appendix B** of the report, which illustrates the functionality of the proposed substandard parking spaces and supportive vehicular manoeuvring diagrams. The updated traffic control signage and pavement marking plan for the underground parking garage in **Appendix D** of the report identifies these spaces as small-car spaces only.



Comment to Loading

In order to improve vehicular and pedestrian safety in the immediate area, a vehicular warning system will be required that informs drivers exiting the underground parking garage that trucks are turning at the top of the primary access ramp and within the internal driveway when the signals are flashing. Furthermore, documentation will be required to indicate how the flashing warning light will be activated when trucks are entering and exiting the loading spaces.

Response to Loading

Noted. Please refer to the updated traffic control signage and pavement marking plans in **Appendix D** of the BA report. A traffic control device specialist will be consulted to determine an appropriate detection system for the site in a future submission.

Comment to Solid Waste Management

Collection will be subject to the following conditions being met:

1. Revised drawings must indicate and annotate a staging pad abutting the front of the Type G loading space that will have an unencumbered vertical clearance of 6.1 metres, constructed of 200mm reinforced concrete and have a grade of no more than 2%.

2. Revised drawings must indicate a bulky storage room of minimum 10 square metres. If the bulk storage is within the waste storage room, it must be annotated.

3. Revised drawings must indicate that all access driveways to be used by the collection vehicle will be level (+/-8%), have a minimum vertical clearance of 4.4 metres throughout, a minimum 4.5 metres wide throughout and 6 metres wide at point of ingress and egress.

4. Revised drawings must indicate that any/all overhead doors the collection vehicle will be passing through have a minimum overhead clearance of 4.4 metres.

5. Revised drawings must annotate that a trained on-site staff member will be available to manoeuvre bins for the collection driver and also act as a flagman when the truck is reversing. In the event the on-site staff is unavailable at the time the City collection vehicle arrives at the site, the collection vehicle will leave the site and not return until the next scheduled collection day.

6. As the planned movement of the collection vehicle is adjacent to exits from the parking garage, revised drawings must indicate a warning system to caution motorists leaving the parking garage of heavy vehicles when loading operations are occurring. This warning system should include both lights and signs.

Response to Solid Waste Management

Please refer to the architectural site plans, and traffic control signage and pavement marking plans, provided in **Appendix A** and **Appendix D**, respectively.





3.0 VEHICULAR PARKING FACILITIES

An updated assessment of the vehicular parking requirement and proposed parking supply is provided in the following sections for 152-164 Bathurst Street and 621-627 Richmond Street West.

3.1 VEHICULAR PARKING REQUIREMENTS

The site is located in a "grey hole" of comprehensive city-wide Zoning By-law 569-2013; as a result, the site is subject to the former City of Toronto Zoning By-law 438-86. Further, a portion of the site is located within the 'MCR' zone. For the purpose of calculating minimum parking requirements for the site, Zoning By-law 438-86 minimum parking requirements are applied to the entire development program. On this basis, the minimum parking requirements for the site are outlined in **Table 2**.

TABLE 2 MINIMUM PARKING REQUIREMENTS – ZONING BY-LAW 438-86 (MCR RATES)

Use	Number of Units / GFA	Requirement	Number of Spaces Required
Bachelor	17		9
1-bedroom	102	0.5 spaces per unit	51
2-bedroom	78	0.75 anagag par unit	59
3-bedroom or more	19	0.75 spaces per unit	14
Resident Sub-total	216	0.62 spaces per unit (blended)	133
Visitors	216	0.06 spaces per unit	13
Retail	528 m ² GFA	None	0
Non-Resident Sub-to	tal		13
TOTAL			146

Notes:

1. Site Statistics based upon stats provided by Kirkor Architects and Planners, dated April 15, 2022.

2. Where a parking requirement results in a number containing a fraction, Zoning By-law 438-86 requires that it be rounded up if the fraction is equal to or greater than 0.5 and rounded down otherwise (except when less than one space).

3.1.1 Proposed Parking Supply

It is proposed to provide vehicular parking in accordance with the minimum parking rates outlined below:

- Residents: 0.24 parking spaces per unit
- Residential Visitors: 0.08 parking spaces per unit (non-exclusive)
- Retail: None, but residential visitor parking supply will be provided on a shared, non-exclusive basis

The proposed resident parking supply rate does not meet the aforementioned Zoning By-law 438-86 MCR parking requirements and therefore, will require a zoning by-law amendment. The proposed residential visitor and retail parking supply meets the appropriate requirements but a technical variance is likely required to permit a commercial parking garage to provide parking for these uses on a shared basis.

The application of the proposed parking supply rates and totals is outlined in Table 3.



Use	Number of Units / GFA	Proposed Parking Rate	Number of Spaces Proposed
Bachelor	17		
1-bedroom	102	0.24 spaces per unit	50
2-bedroom	78	(blended)	52
3-bedroom or more	19		
Resident Sub-total	216	0.24 space per unit	52
Visitors	216	0.08 spaces per unit	18
Retail	528 m² GFA	None	0
Non-Resident Sub-to	tal		18
TOTAL			70
Notes:			

TABLE 3 PROPOSED PARKING SUPPLY

1. Site Statistics based upon stats provided by Kirkor Architects and Planners, dated April 15, 2022.

The application of the proposed parking supply ratios results in a requirement for 70 parking spaces, inclusive of 52 resident parking spaces and 18 visitor parking spaces. As is noted above, it is proposed for the residential visitor parking supply to be provided on a shared non-exclusive basis, meaning that retail visitors will be able to utilize this residential visitor parking supply.

The current application is proposing 1 additional resident parking space (52 spaces) compared to the previous June 2021 ZBA application (51 spaces), which results in a slight increase in the proposed resident parking rate from 0.23 spaces per unit to 0.24 spaces per unit.

The proposed residential visitor parking supply of 18 spaces shared with retail use, equivalent to 0.08 spaces per unit, remains the same between the current SPA #1 application and original ZBA application.

Given that Transportation Services previously accepted the proposed parking supply of 69 spaces, a revised parking supply of 70 spaces (52 resident and 18 shared visitor / retail), it is assumed that the new residential parking rate of 0.24 spaces per unit and commercial parking rate (shared between residential visitor and retail) of 0.08 spaces per unit is reasonably acceptable by the City of Toronto.

3.1.2 Accessible Parking Requirement

Application of the Zoning By-law 579-2017 would require the provision of providing accessible parking spaces for 13 to 100 required parking spaces at a minimum of 1 parking spaces for every 25 parking spaces. The accessible spaces must be adjacent to a 1.5 metre wide accessible barrier free aisle and can be shared between two accessible spaces. The accessible spaces are located in close proximity to the elevator cores and meet the dimensional requirements outlined in Zoning By-law 579-2017.

For the Bathurst / Richmond parking garage, 3 accessible parking 3 are being provided for the overall parking supply of 70 spaces, which meets the minimum Zoning By-law 579-2017 requirement.

A signage and pavement marking plan is provided in **Appendix D** indicating the parking and loading signs.





3.1.3 Toronto Green Standards Version 3.0 – Electric Vehicle Supply Equipment (EVSE) & Low-Emitting Vehicle (LEV) Spaces

For the total 70 parking space supply, it is proposed to provide 14 spaces (20%) with EVSE installed and the remaining 56 spaces (80%) will have provisions for future EVSE installation in compliance with the Toronto Green Standard Version 3.0, standard AQ 1.3.

3.2 SMALL CAR PARKING SPACE REVIEW

3.2.1 Zoning By-law Dimensions

The City of Toronto Zoning By-law 438-86 (as amended by the City of Toronto 494-2007) states the relevant and basic parking space dimensional requirements as follows:

- 2.6 metres in width
- 5.6 metres in length
- 2.0 metres in height
- Accessed by a 6.0 metre drive aisle

Zoning By-law 438-86 (as amended by City of Toronto By-law 494-2007) also specifies that the minimum required width of a parking space shall be increased by 0.3 metres for each side of the parking spaces which is obstructed. The side of the parking space is considered to be obstructed when any part of a fixed object such as, but not limited to, a wall, column, bollard, fence, or pipe is situated within 0.3 metres of the side of the parking space and more than 1.0 metres from the front or rear of the parking space.

Parking spaces that do not meet all of the above requirements will be small car / obstructed parking spaces. Therefore, it is proposed at this time for permission to be established as part of the Zoning By-law Amendment application (and ultimately, in the proposed site-specific by-law for the site) for a maximum of 10% of the total proposed parking supply to be permissible and to count as part of the total parking supply if the parking spaces do not meet all of the above requirements. Further, a maximum of 10% of the total proposed parking supply will be subject to the following parking space minimum dimension requirements:

- 2.4 metres in width
- 5.1 metres in length
- 1.7 metres in height
- the side of the parking space may be obstructed

The majority of parking spaces located within the parking garage of the proposed building comply with the Zoning By-law parking space dimensional requirements. However, 3 spaces out of the proposed 70 parking spaces (equivalent to approximately 4 percent of the total parking supply) do meet the minimum parking dimension standards but are considered side obstructed "small car" parking spaces.

3.2.2 Inventory of "Small Car" Parking Spaces

The reduced architectural Site plans provided in **Appendix A** identify the locations of 3 "small car" parking spaces within the underground parking garage while the small car review plans classify the dimensions and type of obstructed parking space as shown in **Appendix B**. For review purposes, the spaces are referenced in **Table 4**.

Level	Space Label	Space Length (m)	Space Width (m)	Aisle Width (m)	Obstruction
P1	SC-01	5.80	3.00	6.0	One-sided obstructed, wall
P1	SC-02	5.60	2.60	6.0	One-sided obstructed, wall
P2	SC-03	5.80	3.00	6.0	One-sided obstructed, wall

TABLE 4 Small Car Parking Space Inventory

3.2.3 Basis of Small Car Parking Space Review

The functionality of the proposed "small car" parking spaces has been reviewed based upon:

- the ability of these spaces to accommodate a wide range of the vehicle lengths and widths prevalent in the passenger vehicle fleet used today;
- the ability to manoeuvre into / out of the spaces; and,
- the ability for drivers and / or passengers to open car doors in an acceptable manner.

This assessment has been undertaken considering the range of length and widths of the vehicles being used today in an urban centre such as Downtown Toronto.

3.2.4 Design Vehicle

A design vehicle has been selected for the purposes of reviewing the functionality of the 3 "small car" parking spaces. This design vehicle is based upon recent vehicle sales information collected by BA Group and the size of vehicles observed by BA Group at a series of residential buildings within the urban centre of Toronto. The length and width dimensions of the design vehicle used in the small car review spaces reflects a passenger car that is equivalent to the 85th percentile length and width characteristics of the passenger car fleet in use today.

The dimensions of the design vehicle are as follows:

- a) a vehicle length of in the order of 4.97 metres
- b) a vehicle width of in the order of 1.93 metres

These vehicle dimensions reflect, generally, a Ford Explorer sized vehicle.

3.2.5 Analysis of Width Deficient "Obstructed" Parking Spaces

The functionality of a parking space from a width perspective considers obstruction criteria, such as driver / passenger door clearance requirements to allow people to enter and exit a vehicle, once parked in a reasonable manner.

All 3 obstructed "small car" spaces have at least widths of 2.6 metres and lengths of 5.6 metres, which meet the minimum Zoning By-law parking space dimensions. However, these spaces are one-sided obstructed by a wall.

A review of the door opening clearance requirements is provided below.



Door Opening – Clearance Needs

This aspect considers whether the obstructed "small car" spaces provide sufficient clearance to enable vehicle occupants to open their car doors and enter or leave their vehicle. A door opening clearance of between 0.55 metres and 0.65 metres is typically considered as appropriate for use in low to high turnover facilities, respectively. A typical high turnover facility would be a parking lot of a shopping mall, whereas a low turnover facility would be a commuter parking facility or, a primarily residential parking facility such as that provided in this development.

The above suggests that a parking space width of approximately 2.48 metres would be the minimum stall width that would appropriately and adequately provide for a full door opening on one-side of a vehicle (i.e. the driver's side) while allowing a nominal allowance for clearance to any structure on the other side of the vehicle. This is based upon consideration of the 85th percentile design vehicle width discussed in **Section 3.2.4** (i.e. 1.93 metres) and a 0.55 metre door clearance distance.

Each of the obstructed "small car" spaces is 2.6 metres in width, which exceeds the 2.48 metre minimum dimension outlined above. Please note that this assumes the door swing is accommodated within the parking space itself. In many instances, further space is provided within adjacent parking spaces, which could reduce the individual space width needs.

Therefore, all of these obstructed spaces are also considered to be functionally viable spaces. Illustrations of door clearance conditions for the obstructed spaces have also been prepared and are provided in **Appendix B**. These confirm that the proposed obstructed, width deficient spaces are sufficiently wide enough to allow occupants to enter and leave their vehicles in a reasonable manner.

Maximum Small Car Parking Supply Provisions

Recognizing that these 3 one-side obstructed, small car spaces (equivalent to 4 percent of the total parking supply) operate in a reasonable manner and that they accommodate the manoeuvring requirements of a significant portion of the vehicle fleet in Toronto, we suggest that a prospective Site-Specific Zoning By-law permit a maximum of 10 percent of the proposed parking space supply to be small car spaces (equivalent to 7 spaces) which is a reasonable allowance for the City of Toronto.





4.0 LOADING FACILITIES

4.1 LOADING REQUIREMENTS

Application of the minimum loading requirements defined by Zoning By-law 438-86 loading requirements to the proposed site requires one (1) Type 'G' loading space.

4.1.1 Zoning By-law 569-2013

It is noted that if comprehensive City of Toronto Zoning By-law 569-2013 were applicable to the site, an extra Type B loading space would be required because the threshold for the "retail store" use is slightly lower in comparison to Zoning By-law 438-86 (499 m² vs. 550 m²). The proposed retail GFA on the site is 528 m², in the middle of the gap.

4.2 LOADING SUPPLY

One (1) Type 'G' loading space is proposed to support the development proposal. The proposed loading supply satisfies the loading requirements of Zoning By-law 438-86. A signage and pavement marking plan is provided in Appendix D indicating the parking and loading signs.

4.2.1 **Residential Garbage and Recycling Facilities**

Refuse / recycling collection for the residential component of the building will occur within the Type 'G' loading space provided on-site. Appropriate bin staging provisions are provided in front of the Type 'G' loading space in accordance with the design requirements outlined in the "City of Toronto Requirements for Garbage, Recycling and Organics Collection Services for New Developments and Redevelopments" dated May 2012.

Provision for a minimum of 16.6 sq. metres has been provided in front of the Type 'G' loading space to accommodate a total of 4 three cubic yard bins within the allocated area (including 1 bin within the Type 'G' space). The staging areas has been provided in accordance with the City policy requirements (i.e. size of bin staging area = 5 sq. metres for every 50 residential units provided in excess of the first 50 residential units).

Waste storage rooms and uncompacted waste (bulky items) rooms are provided. Provision for a minimum of 77 sg. metres of waste storage and 10 sg. metres uncompacted waste storage has been provided. The waste storage area and uncompacted waste storage area has been provided in accordance with the City policy requirements (i.e. size of waste storage room = minimum 25 sq. metres for the first 50 units plus an additional 13 sq. metres for each additional 50 units and size of uncompacted waste = minimum 10 sq. metres).

4.2.2 Operations and Manoeuvring

Turning movement diagrams have been developed demonstrating the ability for service and delivery vehicles to manoeuvre appropriately within the site when entering / leaving the loading space within the loading area.

The design vehicles used to assess the design arrangements of the proposed loading space includes the City of Toronto garbage collection vehicle, TAC-MSU, TAC-SU, and passenger vehicles.

Updated vehicular manoeuvring diagrams are provided in **Appendix C** to illustrate the turning movements for the design vehicles entering / exiting the proposed loading spaces and confirm that all service vehicles are able to enter / leave the site in a forward motion.

The diagrams confirm that the proposed loading arrangements are appropriate and will facilitate the manoeuvring needs of the vehicles that are expected to make deliveries and collect waste / recycling at the property following the redevelopment as planned.

4.2.3 Height Clearances

The loading areas has been designed such that a minimum height clearance of 4.4 metres is maintained throughout the access driveway and manoeuvring areas leading to / from the loading spaces, which meets the minimum Zoning By-law 438-86 and City of Toronto design standards and policies for height clearance requirements within these areas (i.e. 4.4 metres to access the Type G loading space).

A minimum height clearance of 6.1 metres is provided above the entire Type G loading space and bin staging area to enable compacted bulk lift bin collection services.



5.0 BICYCLE PARKING FACILITIES

5.1 MINIMUM BICYCLE PARKING REQUIREMENT

5.1.1 Zoning By-law 438-86

The site is located in a "grey hole" of comprehensive city-wide Zoning By-law 569-2013. As a result, the site is subject to the former City of Toronto Zoning By-law 438-86. On this basis, the applicable minimum bicycle parking requirement is outlined in **Table 5**; as per Section 4(13)(c), all bicycle parking spaces have been proportionally divided as follows: 80% occupant bicycle parking spaces and 20% visitor bicycle parking spaces.

TABLE 5 ZONING BY-LAW 438-86 BICYCLE PARKING REQUIREMENTS

		Dete	Requirement		
Use	Units / Floor Area	Rate	Occupant	Visitor	
Residential	216 units	0.75 spaces / unit, to a maximum of 200 bicycle parking spaces	130 spaces	32 spaces	
Retail ³	528 m ²	1 space / 1,250 m2 of NFA (if non-residential uses have GFA equal or greater to 2,000 m2)	0 spaces	0 spaces	
TOTAL			130 spaces	32 spaces	

Notes:

1. Based upon the architectural plans provided by Kirkor Architects and Planners, dated April 29, 2021.

The application of Zoning By-law 438-86 minimum bicycle parking requirements to the site results in a requirement of 163 bicycle parking spaces, including 130 long-term bicycle parking spaces and 32 short-term bicycle parking spaces.

Comprehensive city-wide Zoning By-law 569-2013 Zone 1 minimum bicycle parking requirements (which are comparable to the Toronto Green Standard, Tier 1 minimum bicycle parking requirements) have also been applied to the development program in **Table 6**.

TABLE 6 ZONING BY-LAW 569-2013 (TGS) BICYCLE PARKING REQUIREMENTS

11	Units / Floor Area1 Rate tial 216 units Long-term: 0.9 spaces / unit Short-term: 0.1 spaces / unit 2 528 m ² Long-term: 0.2 spaces / 100 m ² of IFA Short-term:	Dete	Requirement		
Use		Long-Term	Short-Term		
Residential	216 units	Long-term: 0.9 spaces / unit Short-term: 0.1 spaces / unit	195 spaces	22 spaces	
Retail ²	528 m²	Long-term: 0.2 spaces / 100 m ² of IFA Short-term: 3 + 0.3 spaces / 100 m ² of IFA	0 spaces	0 spaces	
TOTAL	-		195 spaces	22 spaces	

Notes:

1. Based upon the architectural plans provided by Kirkor Architects and Planners, dated April 15, 2022.

2. Despite the bicycle parking space rates set out in regulations 230.5.10.1(1) and 230.5.10.1(5) and (6), if a bicycle parking space is required for uses on a lot, other than a dwelling unit, and the total interior floor area of all such uses on the lot is 2000 square metres or less, then no bicycle parking space is required.

The application of city-wide Zoning By-law 569-2013 Zone 1 minimum bicycle parking requirements (which are comparable to the Toronto Green Standard, Tier 1 minimum bicycle parking requirements) to the site results in a requirement of 217 bicycle parking spaces, including 195 long-term bicycle parking spaces and 22 short-term bicycle parking spaces.

5.2 BICYCLE PARKING SUPPLY

The architect site statistics indicate a total supply of 223 bicycle parking spaces including 195 residential longterm spaces, 22 residential short term spaces, 3 commercial long-term, and 3 commercial short-term spaces. The overall proposed bicycle parking supply exceeds the requirement of 217 spaces (22 short-term and 195 long-term) defined by the Zoning By-law 569-2013 by 6 spaces (3 long-term spaces and 3 short-term spaces).



6.0 TDM PLAN STRATEGIES

The site context provides for access to existing and planned public transit services and has good pedestrian connectivity, particularly due to its proximity to a variety of land uses. While strong opportunities exist in the area's infrastructure to accommodate sustainable transportation practices, the ability to fully leverage these opportunities is important for ensuring the success of the TDM strategies. To this end, TDM Plan strategies are presented with targeted "intents" (i.e. what it is trying to achieve and for whom), accompanied by methods of implementation.

A summary of applicable mobility strategies is outlined below in **Table 7**. It is important to note that these TDM strategies will be continuously refined throughout the application process. Proposed initiatives based on these strategies are outlined in the following section of this report.

Measure	Intent
Reduce Car Ownership & Usage / Vehicular Parking Supply and Management	 Reduce the need for residents and employees to own a car for occasional/discretionary travel. Reduce the likelihood of privately-owned car use for general travel, particularly during peak periods. Encourage ride-sharing and higher vehicle occupancy. Use parking supply as a tool to reduce automobile travel and support alternate modes.
Enhance Pedestrian Access and Walkability	 Enhance the walkability of the site at-grade and create a truly pedestrian-scaled environment. Improve the quality of the public realm and pedestrian accessibility of the site. Assist in extending a high-quality, safe, accessible, and convenient network of pedestrian linkages that enhance local pedestrian connections to the site and progresses the area-wide pedestrian network. Enhance the ability for residents, employees, and visitors to travel between the site and the surrounding neighbourhoods and transit focal points without the use of a vehicle.
Encourage & Facilitate Bicycle Use	 The provision of physical and operational infrastructure on-site and within the building. Cooperation with the City and other stakeholders, to enhance bicycle connectivity within the area to the broader cycling network.
Encourage Transit Use	 Increase the awareness, utility, practicality and viability of transit travel options for commuter and recreational travel purposes to / from a range of locations across the City and further afield. Enable high-quality and accessible pedestrian connections to the area transit system. Enable the universal use of transit.
Coordination, Communication & Promotion	 Inform and raise awareness of non-automobile travel options for the site. Actively promote non-automobile travel options and services. Introduce, develop and coordinate TDM programs / initiatives with employment / retail tenants within the context of the broader strategies in place for the development as a whole. Enable the successful management of events and special circumstances as they may arise.

TABLE 7 POTENTIAL SITE TRAVEL DEMAND MANAGEMENT PLAN STRATEGIES



6.1 PROPOSED TDM INITIATIVES

Specific TDM initiatives proposed by the developer as part of the mobility strategy to support the proposed development and facilitate use of alternatives to car ownership are outlined below in **Table 8**.

TABLE 8	PROPOSED TDI	M INITIATIVES

Initi	ative	Description
Car	Share Encouragement	
1.	Car-share participation	Local car-share options will be advertised to residents and visitors to the site.
2.	Subsidized Car-share trial memberships	Consideration will be given to partially subsidizing one annual membership with a car-share provider per unit, on request, for one year.
Сус	ling Facilities	
1.	Bicycle parking	As is outlined in Section 5.0 . 223 bicycle parking spaces will be provided including 195 long-term bicycle parking spaces, located in a secure bike storage rooms, and 22 short-term bicycle parking spaces for visitors.
2.	Enhanced cycling maintenance facilities	Bicycle repair stations provided in long-term bike storage rooms.
3.	Encourage Bike Share Toronto to provide a station	The client will work with the City of Toronto and Toronto Parking Authority to locate a Bike Share Toronto station on-site or assist in facilitating the addition of Station to the neighbourhood.
Imp	roved Pedestrian Experience	•
1.	Widened sidewalks	Provision of increased sidewalk width for pedestrian activity at the site frontages for Richmond Street West and Bathurst Street.
Imp	rove Transit Accessibility	
1.	Real-time transit information signage	Indoor signage in lobby with real-time transit information.
Enh	anced Communication	
1.	Multi-modal exterior wayfinding signage	Outdoor multi-modal wayfinding signage
2.	Promotional Events	Upon building occupancy, an event will be scheduled focussed upon alternative transportation options available to new residents in order for them to better make use of the TDM measures outlined in this Plan.
3.	Ongoing TDM marketing	Condominium corporation will be required to promote TDM measures on an ongoing basis.

The combination of the above proposed measures will serve to make travel by transit, walking and cycling easy, and will provide alternatives to parking a car on site for the portion of trips that require the use of a private automobile.



* * * *

We trust the foregoing is satisfactory for the initial Site Plan Approval application for 152-164 Bathurst Street and 621-627 Richmond Street West. Please feel free to contact us should you wish to discuss the above further.

Sincerely, BA Consulting Group Ltd.

Stephen Bahadoor, P.Eng Senior Associate

Stephanie D. Pham, EIT Lead Transportation Designer

*

cc. Ethan Sun, Lead Transportation Analyst Theressa Chung-Hun, Transportation Designer





Appendix A: Reduced Scale Updated Architectural Plans





BATHURST STREET & RICHMOND STREET, TORONTO, ON.

Proposed Mixed-Use Development



DEVELOPER

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HUSSON ENGINEERING + MANAGEMENT 200 CACHET WOODS COURT, T: 905-709-5825 E: greg.rapp@husson.ca CONTACT: GREG RAPP

HYDROGEOLOGY / GEOTECH

PGL ENVIRONMENTAL 102-250 WATER STREET WHITBY, ON. L1N 0G5 T: 905-668-4908 E: pschuster@pggroup.com CONTACT: PAULA SCHUSTER

ENVIRONMENTAL

PGL ENVIRONMENTAL 102-250 WATER STREET WHITBY, ON. L1N 0G5 T: 905-668-4908 E: pschuster@pggroup.com CONTACT: PAULA SCHUSTER

ACOUSTIC ENGINEER

RWDI 600 SOUTHGATE DRIVE GUELPH, ON. N1G 4P6 T: 519-832-1311 E: dan.bacon@rwdi.com CONTACT: DAN BACON

WIND CONSULTANT

RWDI 600 SOUTHGATE DRIVE GUELPH, ON, N1G 4P6 T: 519-832-1311 E: dan.bacon@rwdi.com CONTACT: DAN BACON

LANDSCAPE ARCHITECT

MARTEN-NIKZAD LANDSCAPE ARCHITECTS INC. 1387 BAYNIEW AVE., UNIT 105 SUITE 204, MARKHAM, ON. L6C 0Z8 TORONTO, ON. M4G 3A5 T: 416-245-2299 E: studio@mnlarch.ca CONTACT: AMIR NIKZAD

TRAFFIC CONSULTANT

BA CONSULTING GROUP LTD. 300-45 ST. CLAIR AVENUE WEST TORONTO, ON. M4V 1K9 T: 416-961-7110 E: bahadoor@bagroup.com CONTACT: STEPHEN BAHADOOR

HERITAGE

GBCA 362 DAVENPORT ROAD, SUITE 100 TORONTO, ON. M5R 1K6 T: 416-929-6556 E: nicolas@gbca.ca CONTACT: NICOLAS BARRETTE

SURVEYOR

KRCMAR SURVEYORS LTD. 1137 CENTRE STREET, SUITE 101 THORNHILL, ON. L4J 3M6 T: 905-738-0053 E: tom@krcmar.ca CONTACT: TOM KRCMAR

	DRAWING LIS	т	
Sheet Number	Sheet Name	ZBA Submission - April 29, 2021	ZBA & SPA Submission
12 SITE PL	AN APPROVAL		
dA0.0	Cover Sheet	Yes	Yes
۸1			
dA1.1	Survey	Yes	Yes
dA1.2	Context Map & Statistics	Yes	Yes
dA1.3	Site Plan	Yes	Yes
A2			
dA2.1	Floor Plan - Level P2 & P1	Yes	Yes
dA2.2	Floor Plan - Level 1 & 2	Yes	Yes
dA2.3	Floor Plan - Level 3 & 4		Yes
dA2.4	Floor Plan - Levels 5-6 & 7-9	Yes	Yes
dA2.5	Floor Plan - Levels 9 & 10-17	Yes	Yes
dA2.6	Floor Plan - Mechanical Penthouse & Roof Plan	Yes	Yes
A3			
dA3.1	North & East Elevations	Yes	Yes
dA3.2	South & West Elevations	Yes	Yes
dA3.3	1/50 Elevations West		Yes
dA3.4	1/50 Elevations North		Yes
A4			
dA4.1	Building Sections	Yes	Yes
A5			•
dA5.1	Sun Shadow Study - March / September	Yes	Yes
dA5.2	Sun Shadow Study - June	Yes	Yes
dA5.3	Sun Shadow Study - December	Yes	Yes
A6			
dA6.1	Perspective Views	Yes	Yes
dA6.2	Perspective Views 2		



20-018

Drawing No.

April 15, 2022

dA0

Date:



<form></form>	o, Ontario sed Mixed-Use Development							<u>LOT IN</u>	NFORMATIO	<u>n:</u>
<form></form>	t Statistics 5, 2022					Project No	o. 20-018-11	PLAN ALL O	OF SURVEY F LOTS 1, 2,	′ SHOWING 2A, 3, 4 , 5,
<form></form>	Legal Description PLAN OF SURVEY SHOWING TOPOGRAPHI	CAL INFORMATION OF						PART REGIS	OF RESERV	/E (LYING A N 316 AND
<form></form>	ALL OF LOTS 1, 2, 2A, 3, 4, 5, 6, PRIVATE LA PART OF RESERVE (LYING ALONG WEST S REGISTERED PLAN 316 AND PART OF LOT CITY OF TORONTO	IDE OF PRIVATE LANE AND LOT 6) 2, SECTION I MILITARY RESERVE						CITYC	OF TORONT	0
<form></form>	Site Area							FINISH	IED FLOOR	ELEVATIO
<form><form></form></form>	Gross Site Development Area Total Gross Site Area			hectares 0.1969	acres 0.4866 0.4866	<i>sq.m.</i> 1,969.38 1,969.38	sq.ft. 21,198 21 198	PROP	OSED RESI	DENTIAL:
<form><form><form></form></form></form>	Proposed GFA			0,1303	0.4800	1,303.30	21,190	PROP		RESIDENTI
<form></form>	The gross floor area of a mixed use building is reduced to	y the area in the building used for (A) parking loading and bicycle parking balo	w-ground: (B) required loading spaces at the ground level and required blog	de parking spaces at	or above-ground: (C) storage	rooms, washrooms, electrica	l. utility.			
<form><form></form></form>	Proposed Residential GFA - 17 Storey Condomin floors	ower and change hacilities required by this by-Law for required biotycle parking s nium sq.n	pades, (E) amenity space required by this By-Live, (*) elevator sharts, (d) g N	aroage snans, (n) meo	nanical peninouse, and (/) ex	sq.m.	sq.ft.	NOTE:	<u>S:</u>	
<form><form></form></form>	Level P2 1 x Level P1 1 x Level 1 1 x	34.5 34.5 358.2	2 8 15			34.52 34.59 358.25	372 372 3,856	- FOR		E DETAILS
<form><form></form></form>	Level 2 1 x Level 3 1 x Level 4 1 x	1,191.7 1,271.7 368.1	9 8 18			1,191.79 1,271.78 368.18 2.211.18	12,828 13,689 3,963	ARCH	ITECTS INC.	DRAWING
<form><form><form></form></form></form>	Levels 7 to 8 2 x Levels 9 1 x Levels 10 to 17 8 x	845.4 841.0 744.3	8 18 14			1,690.96 841.08 5,954.72	18,201 9,053 64,096	- FOR MANA	SITE SERV	ICING DETA RAWINGS.
<form><form><form></form></form></form>	Level 18 / MPH 1 x Additional Ammenity over required 1 x Total Residential GFA	413.7 40.2	6 9			413.76 40.29 14,370.79	4,454 434 154,686	- FOR	SURVEY IN	FO PLEAS
<form></form>	Proposed Retail GFA					sq.m. 517.11	sq.ft. 5,566	- FOR	TRAFFIC S	IGNAGE PL
<form><form><form></form></form></form>	Total Retail GFA Grand Total GFA (Residential + Retail)					517.11	5,566	INFOR	RMATION PL	EASE REFE
	Total Residential GFA Total Retail GFA Grand Total GFA					sq.m. 14,370,79 517,11 14,887,90	sq.n. 154,686 5,566 160,252	- SNO	W WILL BE	REMOVED
<form></form>	Proposed Density - FSI					1248.753.45.74			Gen	eral N
<form><form><form></form></form></form>	Unit Count	No of Levels	STIDIO 188	288	380		No. of Units	Г	_	
<form><form><form></form></form></form>	Levels 2 Levels 3	1 x 1 x	2 6 2 7	9 10	0 0	2	17 19		20 De Boers Dr. # 400	ettern + Kaniston D
<form><form><form></form></form></form>	Levels 4 Levels 5 to 6 Levels 7 to 8	1 x 2 x 2 x	0 0 2 6 1 4	1 9 6	2 0 1		3 34 24		Toronto, Ontario M3 Tel: 416-665-6060 Fax: 4 Name of F	U 2K8 116-665-1234 Project: Bathurst & Rich
	Levels 9 Levels 10 to 17 Levels 18 Total Units Count	1 x 8 x 1 x	1 4 1 7 0 4	6 2 1	1 2 1 2 2 1 2 2 2 2 2 2 2 2 2 2 2 2 2 2		12 96 6	-	Location:	152-164 Bathur
<form><form><form><form></form></form></form></form>	Percentage of Total Units		9.0% 46.0%	34.6%	10.4%		100.0%		Item	OBC,
<form><form><form><form><form></form></form></form></form></form>	Vehicular Parking Parking Ratio Proposed						Ratio	ŀ	1 Project Description.	New Residential Apartment
<form><form><form></form></form></form>	Residential Owners Residential Visitors Retail Parking						0.23 /unit 0.080 /unit Zero		2 Major Occupancy(s) Group C - Residentia): Il occupancies
<form><form><form></form></form></form>	Accessible Parking	200.15.10 (1) Parking Rates - Accessible Parking Spaces (A) if t Section 200.15; (B) if the number of requiredparking spaces is	the number of requireparking spaces is less than 13, a minimum of 13 to 100, a minimum of 1 parking space for 25 parking spaces or	Iparking space mu part therof m ust c	st comply with all regulation comply with all regulation	ns for an accessibleparki ns for an accessiblepark sec	ng space in ing space in ction 200.15		Group E - Mercantile Group F3 - Low hazi	occupancies ard industrial occupancies (S
<form><form><form><form></form></form></form></form>	Required Parking Residential Owners		Units 211	Ratio 0.23 /unit		Pari	ting Spaces 48.5		Group A2 – Assembly	रूपुत्र) y occupancies (Amenity Area bazard industrial
<form><form><form></form></form></form>	Residential Visitors Total Parking Required		211	0.08 /unit			16.9 65	-	Building Area (m ²)	nazard maustriar occupance
<form><form><form></form></form></form>	Residential Visitors Residential Visitors Residential Owners Total Accessible Parking Required		Spaces Required 17 49		24		0.7 1.9 3	-	Mezzanine Area (m2) Number of Storeus) Above Grader 18
	Parking Provided Level P2		Owners41	Visitors 0		Pari	sing Spaces 41	-	Number of streets/Fir Building Classification	e Fighter Access 2
<form><form></form></form>	Level P1 Total Parking Provided		10	18 18		Dad	28 69		Group C - Residential Group E - Mercantile	l occupancies occupancies
	Level P1 Total Parking Provided					rar	4 1 5		Group F3 - Medium I 10 Sprinkler System Pro	hazard industnal occupancies posed
<form><form><form></form></form></form>	LEV / EVSE Parking Spaces		Ratio Total Parking Total Parking Provided (6.3) Proposed (6.2)	F	LEV / EVSE Parking Required	LEV Parkin	/ EVSE g Provided		11 Standpipe required 12 Fire Alarm required.	
<form><form></form></form>	EVSE		0.2 × 69		14		14	-	13 Water Service/Supply 14 High Building	is Adequate:
<form><form><form></form></form></form>	Level P2 Level P1						4 10	ŀ	16 Construction Restrict 16 Required Fire Resis	ions:
	Bicycle Parking 230.5.10.1 (5) Bicycle Parking Space Requirements for Di	veiling Units Bicycle parking space requirements for dwelling units in a apartm	ent building or mixed use building are							
<form><form><form><form><form></form></form></form></form></form>	(A) in Bicycle Zone 1, a minimum of 1.0 bicycle parking s Bicycle Parking Ratio Proposed	paces for each dwelling unit, allocated as 0.9 "long-term" bicycle parking spac	e per dwelling unit and 0.1 "short term" bicycleparking space per dwellin	g unit.			Ratio			F
<form><form><form><form><form><form></form></form></form></form></form></form>	Residential Long-Term Residential Short-Term Retail Long-Term Retail Chart Term						0.90 /unit 0.10 /unit (+0.2/100m ²	-	17 DESCRIPTION OF FI	IRE SEPARATIONS
<form><form><form><form><form></form></form></form></form></form>	Bicycle Parking Proposed (T.G.S.) Residential Long-Term		Units / Area 211	Ratio 0.90 /unit		Pari	ting Spaces 190		Exits Stairs Firefighters Elevator 5	Shaft
<form><form><form><form><form></form></form></form></form></form>	Residential Short-Term Retail Long-Term Retail Short-Term		211 517.11 517.11	0.10 /unit 1+0.2/100m ² 1+0.3/100m ³			22 3 3		Electrical Closet (Containing equipmen Electrical Closet	nt required to be in a service r
<form></form>	Total Bicycle Parking Proposed Bicycle Parking Required at Grade		Units	Ratio		Pari	218 sing Spaces		Vertical Service Spac	95
<form><form><form><form><form></form></form></form></form></form>	Bicycle Parking Provided Residential Long-Term		At Grade	Level P1 106	Level P2 84	Pari	ting Spaces		Electrical Equipment	Vault (N/A)
	Residential Short-Term Retail Long-Term Retail Short-Term		22 3 3	0 0	0 0 0		22 3 3		(Including Suite to Co Storage / Locker Roo	rridor Fire Separation (Group ms for Residents
<form><text><form></form></text></form>	Amenity Space (Excluded from GFA)		28	106	84		218		Floor Separations (Including Occupied F	Roof Terraces and Floor-to-Fl
<form><form><form><form></form></form></form></form>	Required Amenity Space 40.10.40.50 (1) <u>Amenity Space</u> In the CR zone, a building with 20 or more dwelling units (A) at least 2.0 matres for each dwelling unit is indoor an	r for Buildings with 20 or More Dwelling Units must provide amenity space at a minimum rate of 4.0 square metres for each o early space. (If at least 40 cause metres is ordinor amenint space in a local	dwelling unit, of which:						Mezzanine Floor Asse Parking Garage to an	embly y Other Occupancy
	and (C) no more than 25% of the outdoor component may a Indoor & Outdoor Amenity Space Required	erny space, (2) a real 400 square menes is buildor ameny space in a rocas be a green roof	rr adjunning or uneury accessore to the incoor amening apace.	Units		Total Amenity R	equired(m2)		Garbage Chute Intake Garbage Chute Disch	e Room sarge Room
	Indoor Amenity Space Outdoor Amenity Space Total Amenity Space		2.00 /unit 40m2 4.00 /unit	211 211			422 40m2 844		Garbage Chute Shaft Central Alarm and Co	antrol Facility
<form><form><form><form></form></form></form></form>	Indoor Amenity Space Provided Level 2 Level 4					sq.m. 72.67 656.43	sq.ff. 782 7.066		Emergency Generato Service Rooms	r Room
<form><form><form><form></form></form></form></form>	Total Indoor Amenity Space Provided Outdoor Amenity Space Provided					729.10 sq.m.	7,848 sq.ft_		Service Rooms (With Fuel Fired Equil	ipment) elevators on balow orade leva
<form><form><form><form></form></form></form></form>	Level 4 Total Outdoor Amenity Space Provided					155.19 155.19	1,670		(1) 2 h if hous	ang emergency life safety cire
<form><form><form><form><form></form></form></form></form></form>	Total Indoor & Outdoor Amenity Space Provided Total Indoor Amenity Space Provided Total Outdoor Amenity Space Provided Total Indoor & Outdoor Amenity Space Provided					729.10 155.19 884.29	7,848 1,670 9,518		(3) 3 hour if n	otores in one into bechag ot sprinklered (3.2.6.7.(1))
<form><form><form><form><form></form></form></form></form></form>	Total Indoor & Outdoor Amenity Ratio Provided					4.19	unit		18 Spatial Separation Refer to code report p	prepared by MGA (Code cons
<form><form><form><form></form></form></form></form>	1								Construction of Exteri Refer to A8 Schedule	ior Walls
<text><text><text><text><text><text><text><text> minite get of the stand of the stan</text></text></text></text></text></text></text></text>	Loading Provided Type 'G' - 13.0m X 4.0m X 6.1m					No	. of Loading		19 Barner-free Design In a Group C major of shall be provided with	ccupancy apartment building, a barrior-free path of travel b
<text><text><text><text><text><text> yiggy of the stand yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yiggy yi</text></text></text></text></text></text>	Building Height									
<text><text><text><text><text><text> Characteriz C Understation Understation Understation Understation Understation<!--/-->Characteriz Understation Understation Understation Characteriz Understation Understation Understation Characteriz Understation Characteriz Characteriz Characteriz Characteriz</text></text></text></text></text></text>	Building Height			18 Storey	.	59.90	\frown			
<section-header><section-header><section-header><section-header><section-header><section-header><section-header><section-header></section-header></section-header></section-header></section-header></section-header></section-header></section-header></section-header>					Stati	stics	6			
<section-header><section-header><section-header><section-header><text><text><text></text></text></text></section-header></section-header></section-header></section-header>							dA1.2			
<text><text><text><text> The star base of the s</text></text></text></text>										
<section-header><section-header><section-header><section-header><section-header><section-header><section-header><section-header></section-header></section-header></section-header></section-header></section-header></section-header></section-header></section-header>		Statistics Template - Toronto Green Standard Version 3.0 Mid to High Rise Residential and all	ء Mic	itatistics Template - To	oronto Green Standard Vers e Residential an	ion 3.0 d all			Statistics T Mid to Hi	emplate - Toronto Green S
<text><text></text></text>	Toronto Green Standard Version 3.0 Statistics T	New Non-Residential Development	Cycling Infrastructure	ew Non-Resid	Proposed Propos	nent	Water Efficiency		New No	n-Residential D
<text><text></text></text>	Plan submitted as part of the applications. Zoning Bylaw Amendment applications: comple	ele General Project Description and Section 1.	Number of short-term bicycle parking spaces (residential) Number of short-term bicycle parking spaces (all other u	es) 3	22 10 3 10	0	fotal landscaped site	area (m²) planted with drougi	ht-tolerant plants	60 30 60
<text><text></text></text>	further information, please visit www.toronto.ca	/greendevelopment	Number of male shower and change facilities (non-resident Number of female shower and change facilities (non-resident	tial) 0 tial) 0	0		minimum 50%) (m² a	as & Soil Volum) B	Required Propose
<form></form>	al Gross Floor Area akdown of project components (m²)	14887.90	Tree Planting & Soil Volume Total Soil Volume (40% of the site area + 66 m ² x 30 m ³).	Required	Proposed Propos	ed %	Fotal site area (m ²) Fotal Soil Volume (405	6 of the site area + 66	i m²x 30 m²)	1969.3 358 90
<form></form>	idential	14370.79 517.11	Section 2: For Site Plan Control App	lications			lotal number of plant lotal number of trees	ing areas (minimum planted	of 30m ³ soil)	1
<form></form>	nmercial	Sett. it	Cycling Infrastructure Number of short-term bicycle parking spaces (all uses) at-grade or on first level below and	Required	Proposed Propos	ed %	Number of surface pa Number of shade tree nterior (minimum 1 to	rking spaces (if appl is located in surface see for 5 parking spa	licable) parking area ices)	n/a n/a
	itutional/Other al number of residential units	211	UHI Non-roof Hardscape	Required	Proposed Propos	ed %	Native and Pollin	ator Supportive	Species	Required Propose
And many frage register in the register in the register interval in the register interval in the register interval interval in the register interval inte	tion 1: For Stand Alone Zoning Bylaw / Plan Control Applications	Amendment Applications and	Total non-roof hardscape area (m ²) Total non-roof hardscape area treated for Urban Heat Isla (minimum 50%) (m ²)	nd 316	632 632 10	0	fotal number of plant	e plants and % of to	tal plants (min.50%)	1.5 3
miser of parking spaces dedicated for priority LEV parking 0 </td <td>Itomobile Infrastructure mber of Parking Spaces</td> <td>Required Proposed % 65 69 106</td> <td>Area of non-roof hardscape treated with: (indicate m²) a) high-albedo surface material</td> <td></td> <td>632 10</td> <td>0</td> <td>Bird Friendly Gla: fotal area of glazing including glass bale</td> <td>ting of all elevations with ony railings)</td> <td>in 12m above grade</td> <td>Required Propose 823.9</td>	Itomobile Infrastructure mber of Parking Spaces	Required Proposed % 65 69 106	Area of non-roof hardscape treated with: (indicate m ²) a) high-albedo surface material		632 10	0	Bird Friendly Gla: fotal area of glazing including glass bale	ting of all elevations with ony railings)	in 12m above grade	Required Propose 823.9
circling first storey of building required Proposed Proposed % a) stade from high-albedo structures a) a) mber of long-term bicycle parking spaces (residurtial) 190 100 a) first storey of building a 100 a) first storey of building a a b) second storey of building a a c) bir level below-ground 106 c) bir level below-ground 84 a) store for Space from Signer provided as Solar Panels (m ³) a) uses tor reside late with the store of space (m ³) a) store for there below-ground 84 a) store for there below-ground 84 b) second storey of building a b) second storey of building a b) second storey of building a c) of there below-ground 84 c) of there below-ground 84 c) of there below-ground 106 c) second storey of building a second storey of building b second storey of building a second storey of building a second storey of building b second storey of building a second storey	mber of parking spaces dedicated for priority L mber of parking spaces with EVSE	EV parking 0 0 0	b) open-grid pavement c) shade from tree canopy				fotal area of treated a glazing within 12m ab	Jazing (minimum 85 ove grade) (m²)	5% of total area of 7	00.32 751.0
mer or iong-term bicycle parking spaces (residential) 190 190 100 mer or iong-term bicycle parking spaces (all other uses) 3 3 100 mer or iong-term bicycle parking spaces (all other uses) 3 3 100 mer or iong-term bicycle parking spaces (all other uses) 3 3 100 mer or iong-term bicycle parking spaces (all other uses) 3 3 100 jo rist user of building 3 3 100 jo scond storey of building 106 106 106 100 <td< td=""><td>rcling Infrastructure</td><td>Required Proposed %</td><td>d) shade from high-albedo structures e) shade from energy generation structures</td><td></td><td></td><td></td><td>a) Low reflectance</td><td>opaque materials</td><td>naue treated with:</td><td></td></td<>	rcling Infrastructure	Required Proposed %	d) shade from high-albedo structures e) shade from energy generation structures				a) Low reflectance	opaque materials	naue treated with:	
Induction on one stand blocked on final data stand on the stand blocked on the sta	mber of long-term bicycle parking spaces (resi mber of long-term bicycle parking spaces (all c	ther uses) 3 3 100	Percentage of required car parking spaces under cover (minimum 75%)(non-residential only)		n/a	and at]	c) Shading			36.87
Solution store of solutioning Solution store of solutioning Solution store of solutioning Solution store of solutioning Solution store of solutioning <td>mber of long-term bicycle parking (all uses) loc a) first storey of building</td> <td>ated on: 3</td> <td>Available Roof Space (m²)</td> <td>Required</td> <td>Proposed Propos 777.27</td> <td>1</td> <td></td> <td></td> <td></td> <td></td>	mber of long-term bicycle parking (all uses) loc a) first storey of building	ated on: 3	Available Roof Space (m ²)	Required	Proposed Propos 777.27	1				
84 e) other levels below-ground 84 Image: Control of the relevel show ground 1 Image: Contrelevel show ground 1 </td <td>c) first level below-ground</td> <td>106</td> <td>Available Roof Space provided as Green Roof (m²) Available Roof Space provided as Cool Roof (m²) Available Roof Space provided as Splar Danals (m²)</td> <td>310.9</td> <td>607.29 78.</td> <td>1</td> <td></td> <td></td> <td></td> <td></td>	c) first level below-ground	106	Available Roof Space provided as Green Roof (m ²) Available Roof Space provided as Cool Roof (m ²) Available Roof Space provided as Splar Danals (m ²)	310.9	607.29 78.	1				
De3 2018-05 Page 1 of 3 Page 1 of 3	a) second level below-ground	84	Construction of a second							
Page 1 of 3							0000 3010 00			
			11-0063 2018-05		Pag	e 2 of 3 11	-0003 2010-05			

2A, 3, 4 , 5, 6, PRIVATE LANE AND

E DETAILS PLEASE REFER TO MARTEN-NIKZAD LANDSCAPE . DRAWINGS.

ICING DETAILS PLEASE REFER TO HUSSON ENGINEERING + RAWINGS.

NFO PLEASE REFER TO KRCMAR SURVEYORS LTD. DRAWING. IGNAGE PLAN, TRUCK TURNING PATH & OTHER TRAFFIC EASE REFER TO BA CONSULTING GROUP LTD. DRAWINGS.

REMOVED FROM SITE.

	20 De Boers Dr. # 400		
	Tel: 416-665-6060 Fax: 416-665-1234		
	Name of Project: Bathurst	& Richmond, Toronto	
	Location: 152-164 B	lathurst Street & 623-627	
		The Ontario	
Item		Data High Rise Mixed	
1.5			
1	Major Occupancy(s)	rtment Bueding	
•	Group C - Residential occupancies		
	Group E - Mercantile occupancies		
	Group F3 - Low hazard industrial occupant	cies (Storage Garage)	
3	Subsidiary Occupancy(s)		
	Group A2 – Assembly occupancies (Amenity	y Areas)	
7922	Group F2 - Medium hazard industrial occup	pancies (Storage and Serv	
4	Building Area (m*)		
6	Mezzanine Area (m2)		
7	Number of Storeys Above Gra	de 18 Be	
8	Number of streets/Fire Fighter Access:	2	
9	Building Classification		
	Group C - Residential occupancies		
	Group E - Mercanble occupancies		
	Group F3 - Medium hazard industrial occup	ancies (Storage Garage)	
10	Sprinkler System Proposed.		
12	Fire Alarm required		
13	Water Service/Supply is Adequate		
14	High Building:		
15	Construction Restrictions:		
16	Paguirad Fire Pagintance Rating (EPP)	Honzontal	
10	required the resistance ranning (rind)	Floors	
		Roof 0	
		Mezzanine 11	
		Floors	
		Roof 0	
17	DESCRIPTION OF FIRE SEPARATIONS	Mezzanne 11	
	Exits Stairs		
	Firefighters Elevator Shaft		
	Electrical Closet	ware more by the Oreland	
	Electrical Closet		
	Vertical Service Spaces		
	Janitor's Room		
	Electrical Equipment Vault (N/A)		
	Suite-to-Suite Fire Separation (Including Suite to Comdor Fire Separation (Group C)		
	Storage / Locker Rooms for Residents		
	Residential Public Corridor		
	Floor Separations Instruction Occurated Reef Terraces and Elever to Elever Ere Separations		
	Floor Separations (Including Occupied Roof Terraces and Floo	r-to-Floor Fire Separations	
	Floor Separations (Including Occupied Roof Terraces and Floo Mezzanine Floor Assembly	r-to-Floor Fire Separation	
	Floor Separations (Including Occupied Roof Terraces and Floo Mezzanine Floor Assembly Parking Garage to any Other Occupancy	r-to-Floor Fire Separation	
	Floor Separations (Including Occupied Roof Terraces and Floo Mezzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room	r-to-Floor Fire Separation	
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	Floor Separations (Including Occupied Roof Terraces and Floor Mazzanne Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Discharge Room Garbage Chute Shaft	r-to-Floor Fire Separation	
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	Floor Separations (Including Occupied Roof Terraces and Floor Mezzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Discharge Room Service Rooms Service Rooms Service Rooms Service Rooms Service Rooms (1) 2 h if housing emergency life saft (2) Willhout Closures at outlid into Di	r-Io-Floor Fire Separations	
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	Floor Separations (Including Occupied Roof Terraces and Floor Mezzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Discharge Room Garbage Chute Discharge Room Garbage Chute Shaft Central Alarm and Control Facility Emergency Generator Room Service Rooms Service Rooms Service Rooms Service Rooms (1) 2 h if housing emergency life saft (2) Without closures at outfoil into Dr (3) 3 hour if not sprinklered (3.2.6.7.	r-Io-Floor Fire Separations de levels ety circuits / equipment scharge Room (1 h otherw (1))	
18	Floor Separations (Including Occupied Roof Terraces and Floor Mezzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Discharge Room Garbage Chute Discharge Room Garbage Chute Shaft Central Alarm and Control Facility Emergency Generator Room Service Rooms (Wan Fact Fred Equipment) Vestibules to protect elevators on below-grad (1) 2 h if housing emergency life saft (2) Without closures at outlot into De (3) 3 hour if not sprinklered (3 2 6 7. Spatial Separation	r-Io-Floor Fire Separations de levels ety circuits / equipment. scharge Room (1 h otherw (1))	
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18	Floor Separations (Including Occupied Roof Terraces and Floor Mazzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Intake Room Garbage Chute Discharge Room Garbage Chute Shaft Central Alarm and Control Facility Emergency Generator Room Service Rooms Service Rooms Service Rooms (1) 2 h if housing emergency life saft (2) Without closures at outlot into De (3) 3 hour if not sprinklered (3.2.6.7. Spatial Separation Refer to code report prepared by MGA (Cod Construction of Edetor Walls Rafer to A& Schedule	r-to-Floor Fire Separations de levets ety circuits / equipment. scharge Room (1 h otherw (1)) e consultant)	
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18	Floor Separations (Including Occupied Roof Terraces and Floor Mazzanine Floor Assembly Parking Garage to any Other Occupancy Garbage Chute Intake Room Garbage Chute Discharge Room Garbage Chute Discharge Room Garbage Chute Shaft Central Alarm and Control Facility Emergency Generator Room Service Rooms (With Fuel Fined Equipment) Vestibules to protect elevators on below-grad (1) 2 h it housing emergency life saft (2) Without closures at outlet into De (3) 3 hour if net sprinklered (3.2.6.7. Spatial Separation Refer to code report prepared by MGA (Code Construction of Exterior Walls Rafer to A& Schedule Bartiser-frike Design In a Group C mayer occupancy apartment be	r-to-Floor Fire Separations de levels de levels ety circuits / equipment: scharge Room (1 h otherw (1)) e consultant) aiding, not less than 15% (

Page 3 of 3 G.S. / 5 \dA1.2

SHOWING TOPOGRAPHICAL INFORMATION OF /E (LYING ALONG WEST SIDE OF PRIVATE LANE AND LOT 6) AN 316 AND PART OF LOT 2, SECTION I MILITARY RESERVE

F.F.E. = 90.60 -RESIDENTIAL (COMMERCIAL): F.F.E. = 90.00; 90.25; 90.55

NOTES:

TOTAL REQUIRED STAGING AREA: (211-50) / 50 x 5m² = 16.1m²

5m² FOR EVERY 50 UNITS > 50

GARBAGE/RECYCLING:

CITY OF TORONTO REQUIREMENTS (MAY 2012):

 $25m^2 + (13m^2 x ((211-50)/50)) + 10m^2 = 76.86m^2$

CALCULATED STAGING AREA REQUIREMENT:

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ECUMSI

-

PAVEMENT DESIGN OF ACCESS ROUTE SHALL MEET THE FOLLOWING DEPTH REQUIREMENTS:

- 50MM COMPACTED DEPTH HL-3 ASPHALT FOR TOP COURSE - 75MM COMPACTED DEPTH HL-9 ASPHALT FOR BASE COURSE

- 150MM COMPACTED DEPTH OF 25MM Ø CRUSHER RUN LIMESTONE

- 300MM COMPACTED DEPTH OF 50MM Ø CRUSHER RUN LIMESTONE

DRIVEWAY WIDTH SHALL BE A MINIMUM 6.0 METRES FROM FACE-OF-CURB TO FACE-OF-CURB

RADIUS THROUGHOUT ENTIRE ACCESS ROUTE SHALL BE NO LESS THAN 12.0 METRES (CENTRE LINE).

ACCESS ROUTE TO HAVE MNIMUM VERTICAL CLEARANCE OF 4.4M AND SLOPE SHALL NOT BE GREATER THEN 5%.

STRUCTURE BELOW CAN SAFELY SUPPORT A FULLY LOADED COLLECTION VEHICLE WEIGHING 35,000KG, AND SHALL CONFORM ALL APPLICABLE LEGISLATION.

LOADING AREA AND LOADING PAD TO HAVE MINIMUM VERTICAL CLEARANCE OF 7.5M.

LOADING PAD SHALL HAVE A MINIMUM BASE OF 300MM COMPACTED 20MM CRUSHER RUN LIMESTONE AND SHALL BE

FINSIHED TO A MINIMUM OF 200MM DEPTH OF CONCRETE OR A CITY APPROVED ALTERNTIVE.

GRADE OF LOADING PAD SHALL BE NO GREATER THAN ±2%.

BOLLARDS OR OTHER TYPE BARRIERS AREA TO BE INSTALLED ON EITHER SIDE OF THE LOADING DOOR(S).

SNOW STORAGE AREAS MUST NOT INTERFERE OR COMPROMISE THE MINIMUM SPECIFICATIONS OF THE ACCESS ROUTE OR TURNING OPERATIONS.

RESPONSIBILITY OF OWNER TO MEET NO LESS THAN MINIMUM STANDARDS POURSUANT TO ONTARIO BUILDING CODE AND APPROPRIATE ODOUR CONTROLS REQUIREMENTS FOR WASTE STORAGE FACILITY.

Waste Management Notes 3

 $\langle dA1.2 \rangle$

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RST STREET	ARCHITECTS BRIENT FRANKLIN WHITBY CENCE 6723
INTER ARCH	IRKOR HITECTS AND PLANNERS 20 De Boers Drive Suite 400 Toronto, ON M3J 0H1
	sion: Date:
Image: Context Map 1	
1 : 1000 UAT.2 2 ZBA 1 Zoni No: Issue	Resubmission & 1st SPA April 15, 2022 ng By-Law Amendment #1 April 29, 2021 ed For: Date:
	Client: Originate Developments 152-164 Bathurst Street & 623-627 Richmond Street, Toronto
Con	Proposed Residential Development Drawing Title: text Map & Statistics
	Scale: 1 : 1000 Drawn by: A.E Checked by: R.P Project No.: 20-018 Date: April 15, 2022
Context 3D Massing 2 dA1.2	dA1.2

*Toronto, Front Loading, Feb 2016

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	Date: ASSOCIATION ARCHITECTS BRENT FRANKLIN WHITBY CENCE 6723 CONTROL 2022-04-25
	KIRKOR ARCHITECTS AND PLANNERS 20 De Boers Drive Suite 400
	No.: Revision: Date:
	2 ZBA Resubmission & 1st SPA April 15, 2022
	1Zoning By-Law Amendment #1April 29, 2021No.:Issued For:Date:
	Client:
	152-164 Bathurst Street & 623-627 Richmond Street, Toronto Proposed Residential Development
	Drawing Title: Site Plan
	N Scale: 1 : 200 Drawn by: A.E Checked by:
	R.P Project No.: 20-018 Date: April 15, 2022 Drawing No.:
Site Plan 1 1 : 200 dA1.3	dA1.3

		Proposed
Gross Floor Area, as defined in Green Roof Bylaw (m ²)		14887.9
Total Roof Area (m ²)		1322.82
Area of Residential Private Terraces (m ²)		512.10
Rooftop Outdoor Amenity Space, if in a Residential Building (m ²)		33.45
Area of Renewable Energy Devices (m ²)		0
Tower (s)Roof Area with floor plate less than 750 m ²		
Total Available Roof Space (m ²)		777.27
Green Roof Coverage	Required	Proposed
Coverage of Available Roof Space (m ²)	310.9	607.29
Coverage of Available Roof Space (%)	40	78,13

Appendix B: Small Car Review

Drawing No.

SC-03

Appendix C: Vehicular Manoeuvring Diagrams

INBOUND:

INBOUND:

PUDO SPACE #2:

INBOUND:

INBOUND:

Appendix D: Signage and Pavement Marking Plans

SIGN LEGEND R SMALL CAR R R LOADIN PARKING ONLY Rb-51 (300 x 300) LZ (300 x 450)mm EV PARKING Small Car Only (300x 450)mm (300 x 450)mm Rb-93 (300 x 450)mm AUTHORIZED PARKING OMMS ROOM TANK AUTHORIZED P (450 x 600)mm • PREPARE TO STOP WHEN FLASHING • ARKING SPACES TOTAL LEVEL 🗗: 28 F 18 VISI ESIDENT WARNING SIGN CAUTION: TRUCK MANOEUVRING AHEAD WARNING SIGN SIGN MOUNT LEGEND ALL SIGNS ARE SHOWN IN APPROXIMATE LOCATIONS AND TO BE DETERMINED ON SITE. SIGNS MUST BE VISIBLE TO DRIVER AND NOT NO PARKING OBSTRUCTED BY LANDSCAPE. ð FLEXABLE POST SIGN POST 94 6.00 n 24 FLEXABLE POST SIGN CONVEX MIRROR ¥6 24 SUSPENDED SIGN 6.00 m -AUTHORIZED PARKING ONLY SIGN Ÿ٤ 24 PAVEMENT MARKING LEGEND ۷ć NOTE-ALL MARKINGS MUST CONFORM TO THE ONTARIO TRAFFIC MANUAL (OTM) BOOK 11 ٧¢ RAMP DN TO LEVEL P2 ·Ε YELLOW HATCHING V/// (10cm WIDE YELLOW STRIPS) ۲¢ ELECTRIC VEHICLE PARKING -- 1.5m x 1.5m INTERNATIONAL SYMBOL OF ACCESS - 1.5m x 1.5m - 10cm BORDER - TRAFFIC WHITE - SYMBOL - TRAFFIC WHITE - BLUE BACKGROUND 8 20m 0 2 4 6 10 1:400 Scale 623 RICHMOND STREET WEST Project: 623 Richmond St W Project No. 7978-02 Pavement Marking and Signage Plan Date: April 5, 2021 **BA** Group P1 Level April 27, 2022 Revised: **SN-02** Drawing No.

